

Appendix 7: Level 3 Building Recording of
The Transport Building

Blue Bird Park
Bromsgrove Road
Hunnington
Worcestershire
B62 0JW

National Grid Reference: SO 96570 81441
Planning Reference Number: 19/00592/FUL

Dr Peter Wardle & Colin Lacey

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Version 1

<p>Appendix 7 Building Recording of: Transport Building Blue Bird Park Dr Peter Wardle & Colin Lacey</p>
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Version Control

Version No	Draft	Content Added/Omitted	Date
0.5	Client Draft		24/4/23
0.7	LPA Draft	Client Comments	
1	Issued Report		2 5 2023

Heritage Statement: Transport Building Dr Peter Wardle & Colin Lacey

1. Summary

Building 7 has been identified as a former transport building from aerial photographic evidence, signage and building form. The building has subsequently been used as offices and for storage and repair of motorcycles. This building has now been demolished.

Current Function	Vacant
Original Function	Transport Building
Period	Modern
Century	20 th
Precise Date	1925-7
Architectural Style	Utilitarian
Architect	S N Cooke FRIBA
Materials	Brick, concrete
Designation	Not Listed
Conservation Area	Not located within a conservation area

2. Introduction

2.1 The Building

The building is a former transport building – a purpose-built office and vehicle garaging for the Blue Bird toffee factory.

2.2 The Client

This report was commissioned J Richards of Seven Capital (Halesowen) Ltd. and subsequently Malvern Estates Ltd.

2.3 Copyright

The copyright of this report belongs to the Historic Environment Consultancy. No liability to third parties is accepted for advice and statements made in this report.

2.4 Location

Site Address:	Transport Building Blue Bird Park Bromsgrove Road Hunnington Worcestershire
Post Code:	B62 0JW
Grid Reference:	SO 96565 81441
Latitude and Longitude	52.430937 , -2.0519478

The general location is shown in Figure 1 and the detailed location in Figure 2.

2.5 Date of Recording

The building was initially visited by Dr Peter Wardle in July & October 2019. Subsequent visits were made by Dr Wardle and Colin Lacey between this point and September 2022.

2.6 Circumstances of The Project

The recording of the building was required as a condition of a grant of planning permission and listed building consent reference Bromsgrove Borough Council 19/00592/FUL for:

“Part demolition and site clearance of the former Blue Bird factory site for its redevelopment to provide 116 residential dwellings (Use Class C3), consisting of both new dwellings and conversion of the Welfare and Administration buildings, along with associated landscaping; drainage; engineering; highways and access works”.

The wording of the planning condition is as follows:

No development shall commence until a programme of research and building recording including a Written Scheme of Investigation(s), has been submitted to and approved by the local planning authority in writing. The scheme shall include an assessment of significance and research questions; and:

- a) The programme and methodology of site investigation and recording which will include:*
- 1. A Level 3 historic building recording (as defined by Historic England) on all pre 1980s buildings. This will include documentary research.*
 - 2. A Level 1 historic building recording (as defined by Historic England) on warehouses, dating to the 1980s.*

3. *Investigative works prior to and during construction works to recover structural information.*

b) *The programme for post investigation assessment.*

c) *Provision to be made for analysis of the site investigation and recording.*

d) *Provision to be made for publication and dissemination of the analysis and records of the site investigation*

e) *Provision to be made for archive deposition of the analysis and records of the site investigation*

f) *Nomination of a competent person or persons/organisation to undertake the works set out within the Written Scheme of Investigation.*

REASON: In accordance with the requirements of paragraph 194 of the National Planning Policy Framework.

2.7 Location of The Archive

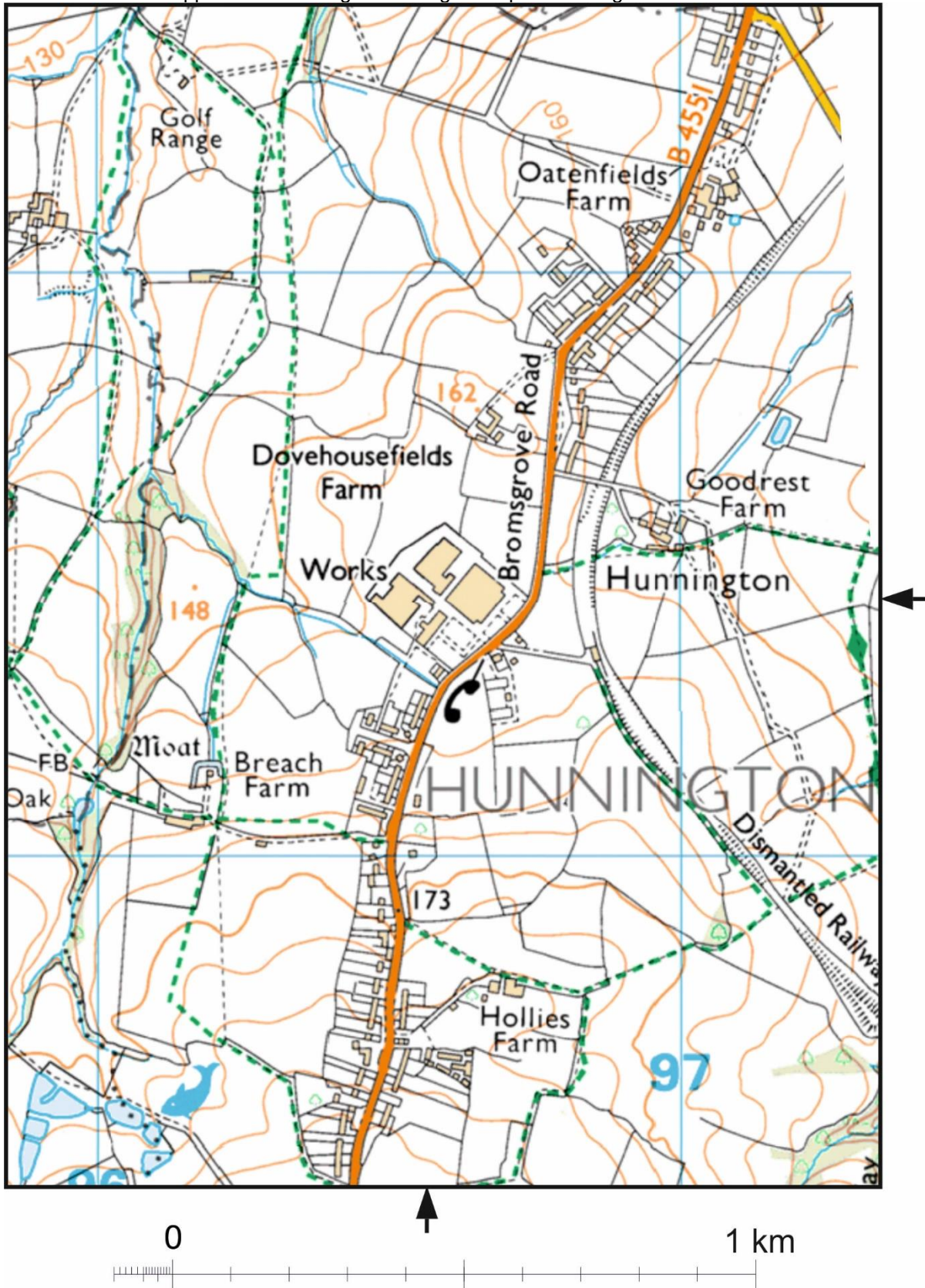
The archive of this project consists solely in the form of digital records. This report contains 667 high resolution photographs and 40 plans and elevations. The drawings are appended to the report as layered vector files. Copies of the report in pdf archive format therefore represent the archive.

Copies of this archive are therefore given to the Archaeology Data Service, Worcestershire County Council Historic Environment Record and Worcestershire Archive & Archaeology Service or Worcestershire County Museum.

2.8 Methodology

The building was recorded according to the requirements set out in the English Heritage 2016 *Understanding Historic Buildings A Guide to Recording Practice* and the method statement.

There were no difficulties experienced in the recording process.



Metres 1:10,000 @ A4

Figure 1: General Location Plan. Scale 1:10,000

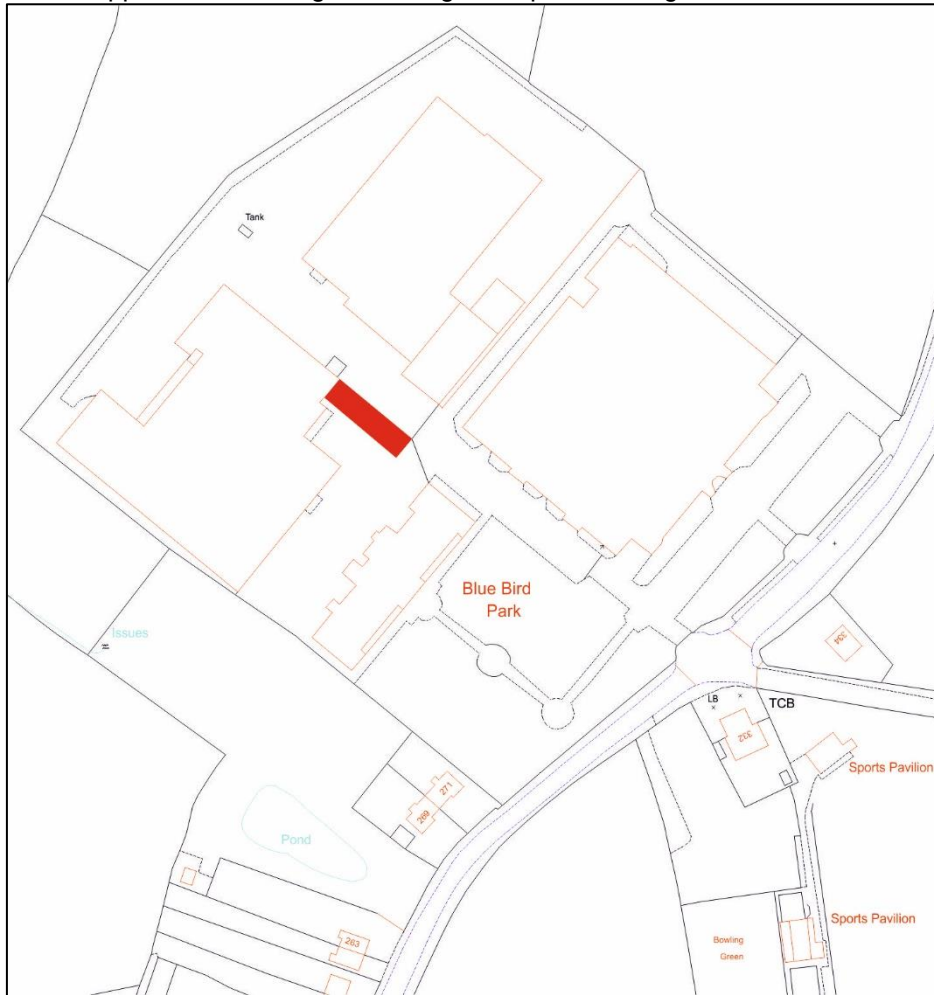


Figure 2: Detailed Location Plan. Scale 1:2500

3. Historic Background

3.1 Documentary Evidence

An article was published in *The Midlander* in February 1927. The article details the background behind the Blue Bird factory and the village of Hunnington describing the history of the Messrs Harry Vincent Ltd and Harry Vincent, himself.

Whilst this article does not mention the transport building, it has a sketch of the factory complex from the road. This shows the transport building. An excerpt of the drawing is shown below.



Figure 3: Detail of transport building from 1927 Midlander article

A newspaper article from the *Birmingham Gazette*, published on the 13th May 1930, concerning the factory does not mention the transport building.

3.2 Historic Photographs



Plate 1: 1936 aerial view, Britain From Above



Plate 2: 1936 aerial view, Britain From Above



Plate 3: 1936 aerial view, Britain From Above



Plate 4: 1948 aerial view, Britain From Above



Plate 5: 1948 aerial view, Britain From Above

4. The Building

4.1 The Building

Transport building for the Blue Bird toffee factory.

Current Function	Motorcycle warehouse / repair shop
Original Function	Transport Building
Period	Modern
Century	20 th
Precise Date	1925-7
Architectural Style	Utilitarian
Architect	S N Cooke FRIBA
Materials	Brick, concrete
Designation	Not Listed
Conservation Area	Not located within a conservation area

4.2 The Importance of the Building Listing Criteria

Date		For modern buildings one of the other criteria has to be clearly met.
Rarity		Industrial Buildings are a common type of historic building.
Architectural Interest		
	Architect	The building was designed by a prominent architect.
	Type	There is little particular interest in the building type.
	Innovation	This building does not use innovative building techniques or materials.
	Virtuosity	The building does not demonstrate a particularly high standard of craftsmanship.
	Plan Form	The plan form is not significant.
Historic Interest		This building does not illustrate important aspects of the nation's history. It is not associated with a Nationally Important person.
Group Value		There is group value with the other historic buildings.
Contribution of Setting		The building is surrounded by buildings of different dates and therefore there is no added importance from the setting.

4.3 The Importance of the Building Understanding Heritage – Values Conservation Principles Policies and Guidance 2008

Evidential		The building dates from a period when the physical evidence of the building adds little novel information.
Historical		
	Illustrative	Low The building does not illustrate an unusual historic connection.
	Association	Low The building is not associated with a notable person, family, event, movement or cultural heritage.

Aesthetic		
	Design	The building is a utilitarian building and design is not a factor.
	Detailing & Craftsmanship	The building does not demonstrate a very high standard of craftsmanship.
	Architect	The building was designed by a prominent architect.
	Innovation	This building does not use innovative building techniques or materials.
Communal		Low – The building is visible from the public realm so it does have a communal value albeit a very low one.

4.4 Designations

The building is not listed.



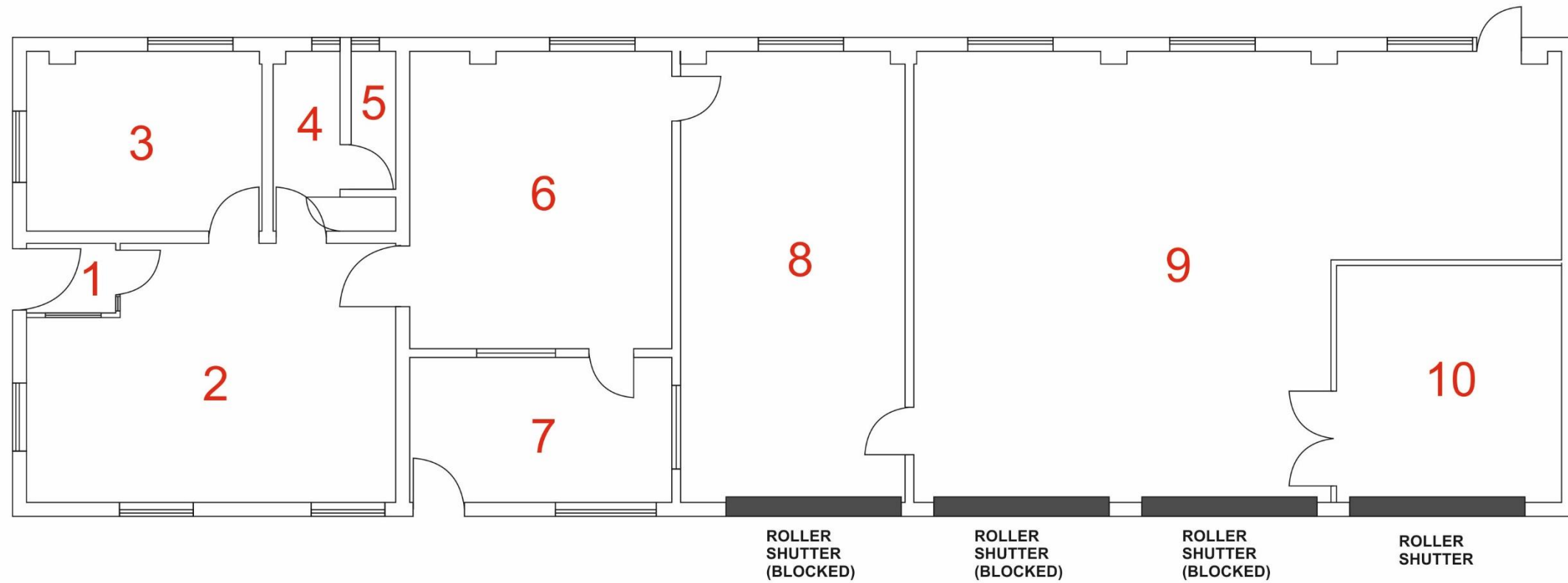
Plate 6: The Building



Plate 7: The Building

4.5 The Layout of The Building

The layout of the building is shown overleaf.



Blue Bird Park
Transport Building Floor Plan



Scale @A3 1:100



Figure 4: Floor Plan

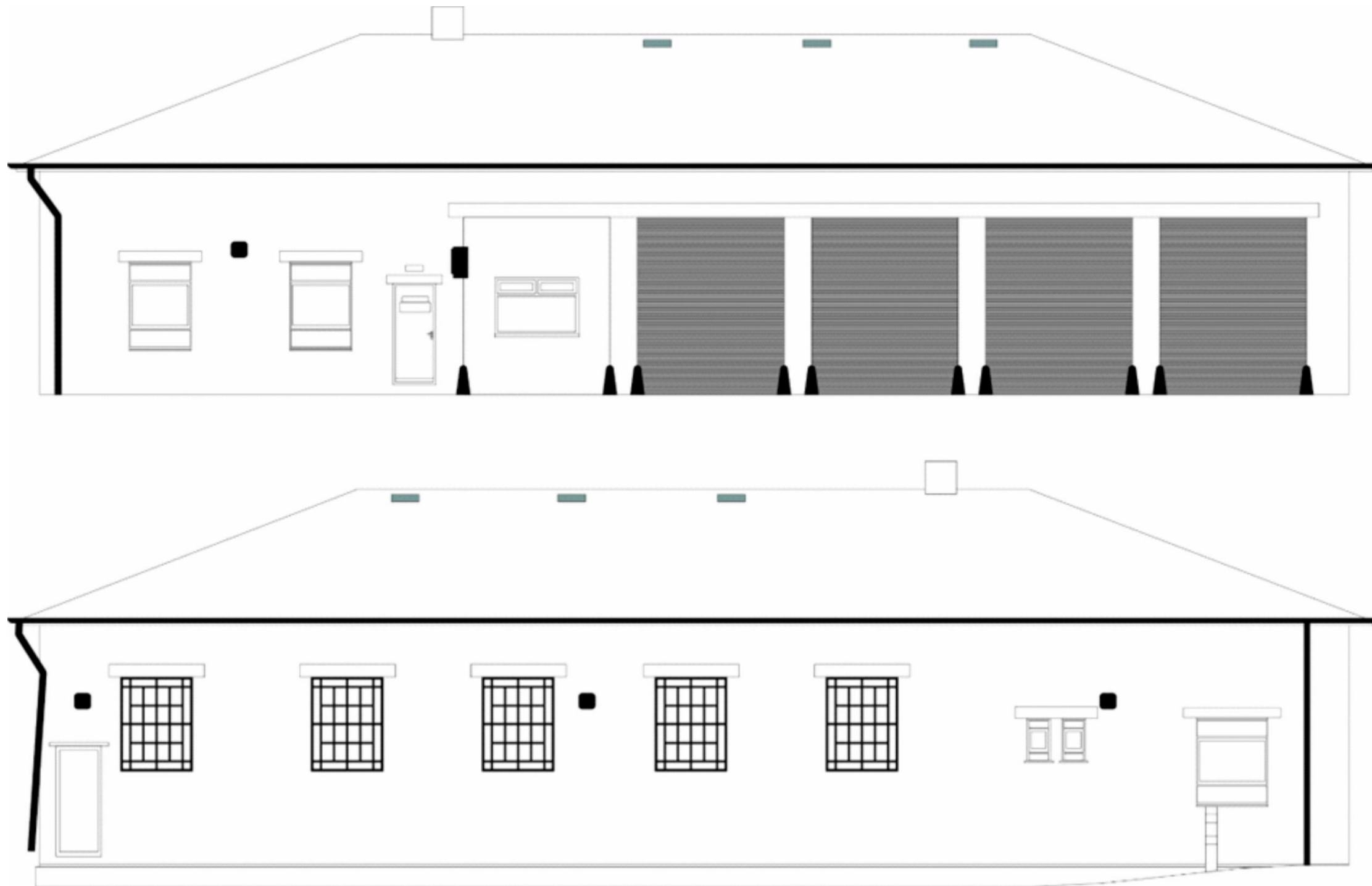


Figure 5: Northeast and southwest elevations, 1:100 @ A3

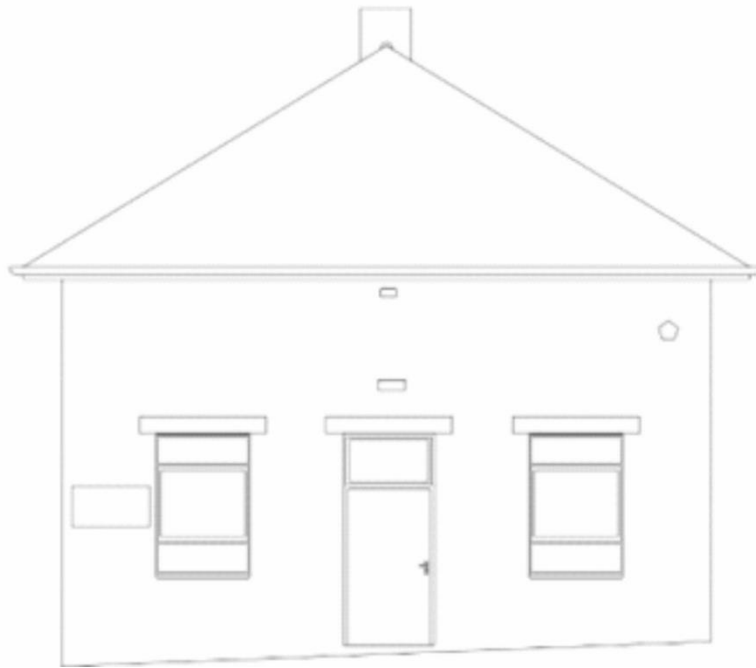


Figure 6: Southeast elevation, 1:100 @ A3

4.1 The Date of the Building

The building was built in the years between 1925 and 1927.

4.2 The Construction Sequence

The following Phases of construction are present:

Phase	Period	Date	Description	How Dated
1	Modern	1925-7	Main structure of building	Historic mapping and photographs
2	Modern		Internal alterations and subdivisions	Plan form

Phased plans are reproduced below.

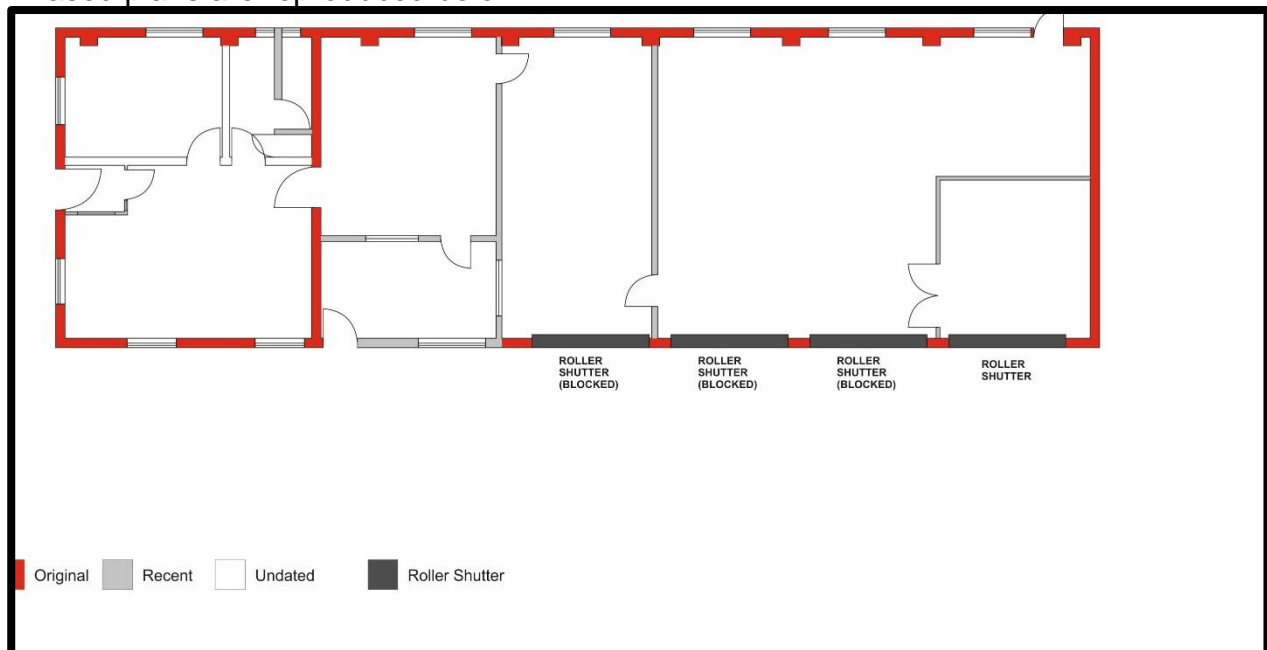


Figure 7: Phase Plan

4.3 The Plan Form of The Building As Originally Built

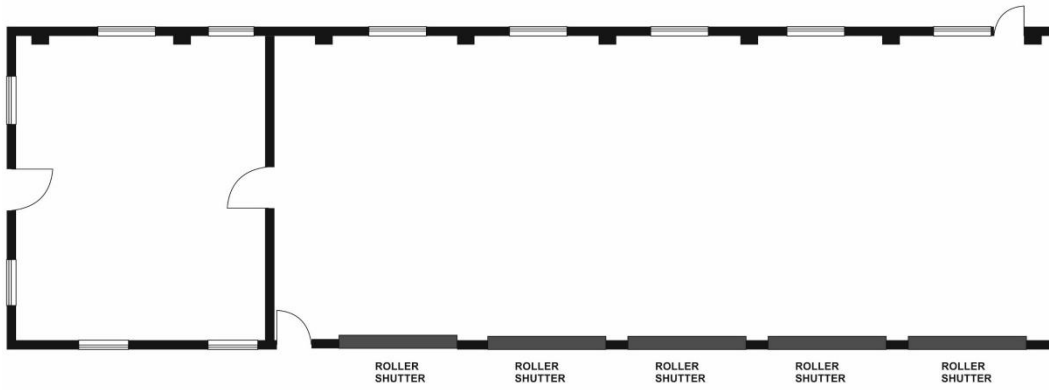


Figure 8: The layout of the building as originally built 1:1200

5. Description: Exterior

The transport building is a northwest-southeast oriented building of red brick in the same bond based on stretcher, Flemish and English Garden Wall bonds as is found in the main manufactory building, with the addition of a course of vertical headers low to the wall.

The roof is a low hipped structure with a small chimney towards the southeast end of the building. Small roof lights / vents are present to the northwest.



Plate 8: Northeast elevation

The transport building was originally where vehicle movements were arranged and where vehicles were stored. The building featured administrative facilities at its southeast end, with five vehicle bays to the northwest.

To the left, a single pedestrian door is present, followed by two windows. The door has a sign for the 'vehicle reception', relating to the former use of the building. To the right, four roller shutter doors are present. A recessed brick wall with window shows where a fifth roller shutter was once located.



Plate 9: Southeast part of northeast elevation



Plate 10: Northwest part of northeast elevation



Plate 11: Link between transport building and building 11

A flat-roofed single-storey structure connects the transport building with building 11. The presence of louvred doors and warning signs suggests the building contained electrical distribution equipment.



Plate 12: Detail of window, northeast elevation



Plate 13: Signs indicating the previous use of the building



Plate 14: Detail of door and window

A central door in the southeast elevation is flanked by three-paned windows. All openings have concrete lintels. The door has a fixed light above, labelled 'RECEPTION'.



Plate 15: Southeast elevation



Plate 16: Detail of southeast window



Plate 17: Detail of southeast door

A single timber door is present to the left of the southwest elevation, followed by five steel-framed multi-pane fixed windows. A pair of smaller steel-framed single windows and a larger three-pane window (both at a lower level to the multi-pane windows), are present to the right. All windows have concrete lintels.



Plate 18: Southwest elevation



Plate 19: Detail of door and window, northwest end of southwest elevation



Plate 20: Detail of window, southwest elevation



Plate 21: Detail of windows, southwest elevation



Plate 22: Detail of window, southwest elevation



Plate 23: Detail of chimney



Plate 24: 2022 Aerial View of Transport Building (survey commissioned for this exercise)

6. Description: Interior

A modern suspended ceiling is present throughout the building, obscuring any details of the structure above.

6.1 Room 1: Entrance Lobby



Plate 25: Northwest aspect, room 1

The entrance lobby is accessed via the southeast door. A glazed timber door opens into the reception. A window is present also opening into the reception.

6.2 Room 2, Reception



Plate 26: Northwest aspect, reception

The reception features a wooden reception desk / counter in front of the entrance door. Doorways lead off to the northwest and southwest.



Plate 27: Southwest aspect, reception



Plate 28: Southeast aspect, reception

6.3 Room 3

Room 3 is located in the southern corner of the building, to the northwest of the reception. There are no features of historic interest in this room.



Plate 29: Southwest aspect, room 3



Plate 30: Northwest aspect, room 3



Plate 31: Southeast aspect, room 3

6.4 Room 4: Kitchen

A small works kitchen is present in room 4. The kitchen is considered to have been converted / partitioned from a lavatory once the canteen building had ceased to provide catering facilities. Modern kitchen fittings and a modern boiler are present.



Plate 32: Southwest aspect, kitchen



Plate 33: Northeast aspect, kitchen

6.5 Room 5: Lavatory

The lavatory is accessed via a door in the northwest wall of the kitchen. There are no historic features present.



Plate 34: SW aspect, lavatory

6.6 Room 6



Plate 35: Northwest aspect, room 6

Room 6 is located to the southwest of the reception.



Plate 36: Northeast aspect, room 6

The room has an internal door and window to room 7, and it is considered that the room was used as an office supervising deliveries and other transport activities with drivers arriving at the internal window from room 7 to communicate with office staff.



Plate 37: Southeast aspect, room 6



Plate 38: Southwest aspect, room 6

A small cupboard built into the western corner of the room. This is comparatively recent in construction and cables internally suggest it has been used to house electrical / data equipment.

There are no visible historic features in room 6.

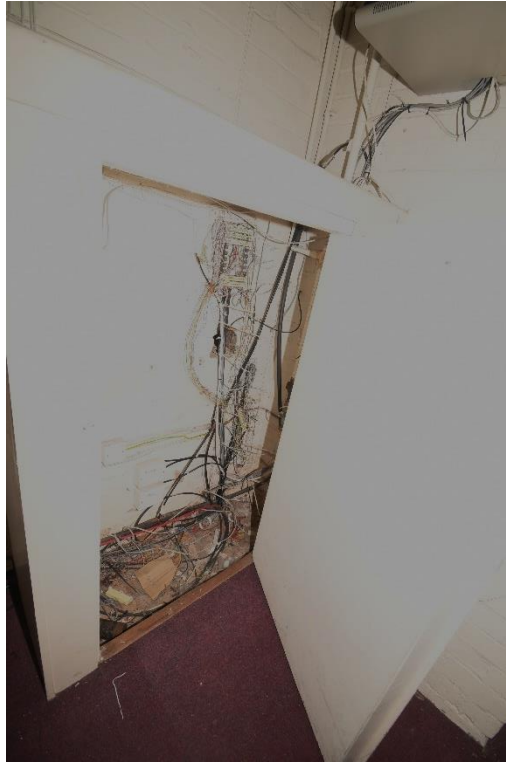


Plate 39: View into cupboard

6.7 Room 7



Plate 40: Southeast aspect, room 7

Room 7 has an external door in its northeast wall and an internal door to room 6 in its southwest wall. This was previously used as a reception area where drivers would receive their instructions for the day via the window to room 6.



Plate 41: Northwest aspect, room 7

6.8 Room 8

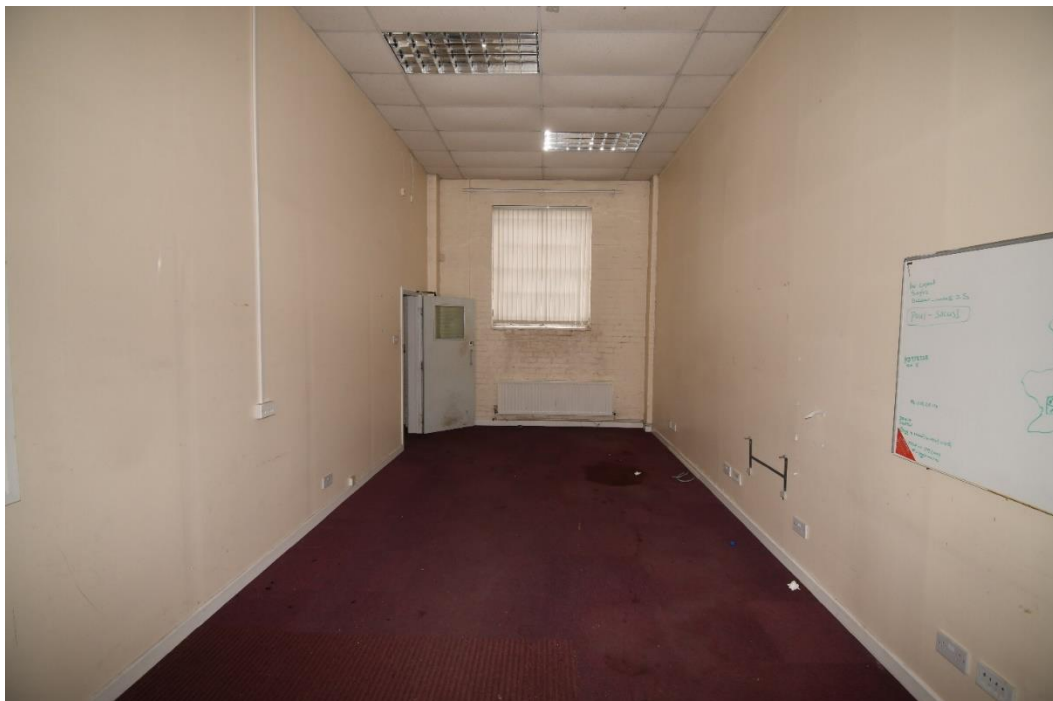


Plate 42: Southwest aspect, room 8

This room is located in a former vehicle bay of the transport building. The former opening to the northeast is filled with brick and a window. Externally, a concrete lintel is present showing its original form.



Plate 43: Northeast aspect, room 8

6.9 Room 9



Plate 44: Northwest aspect, room 9

Room 9 has minimal decoration. A suspended ceiling is present, however the walls are of bare brick, painted white. The floor is of concrete with no carpet. A modern stud wall divides this area from room 8.

Room 9 comprises two former vehicle bays. A block wall has been constructed internally over the two roller shutters, which are still fitted and visible externally. The roller mechanism has been panelled off internally although the chains for raising and lowering the shutters are still fitted.



Plate 45: Southwest aspect, room 9

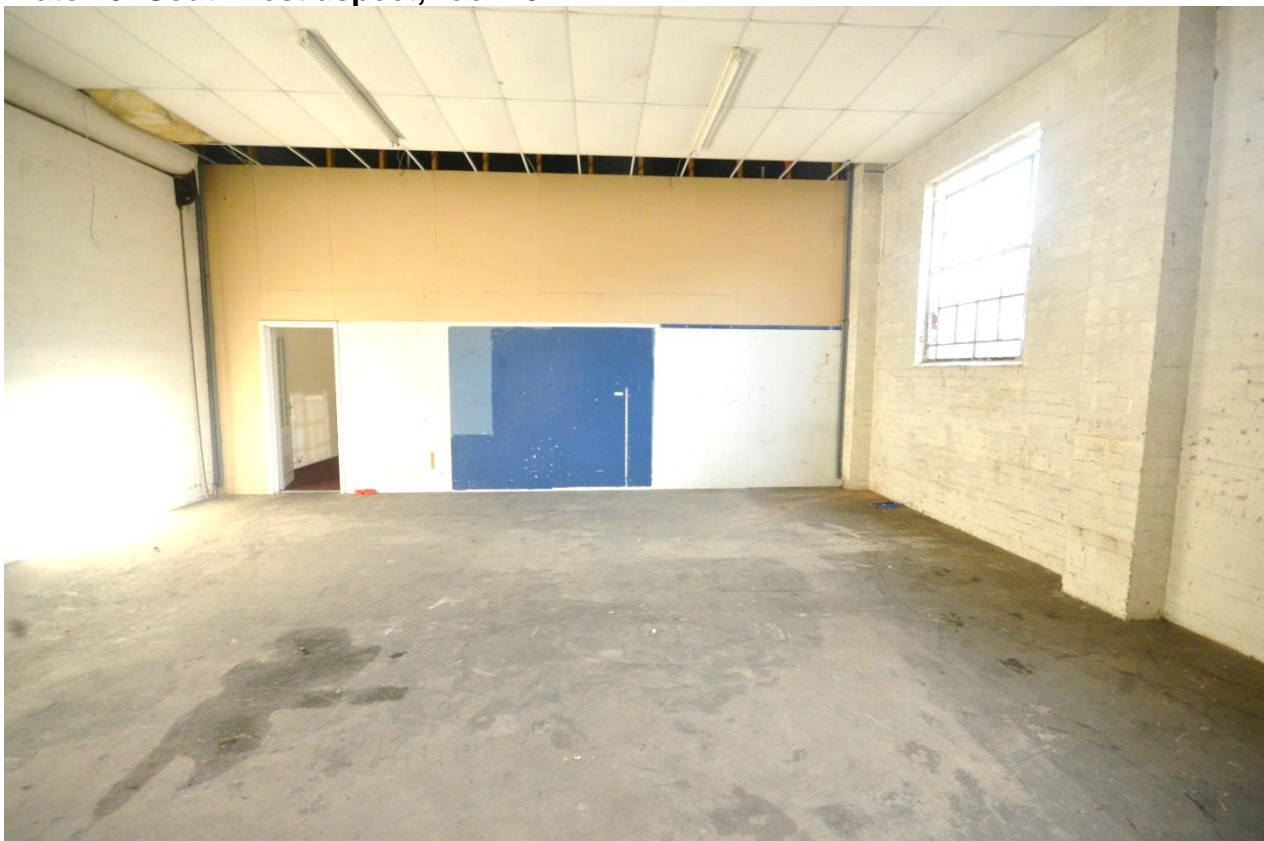


Plate 46: Southeast aspect, room 9



Plate 47: Southwest aspect, room 9

6.10 Room 10

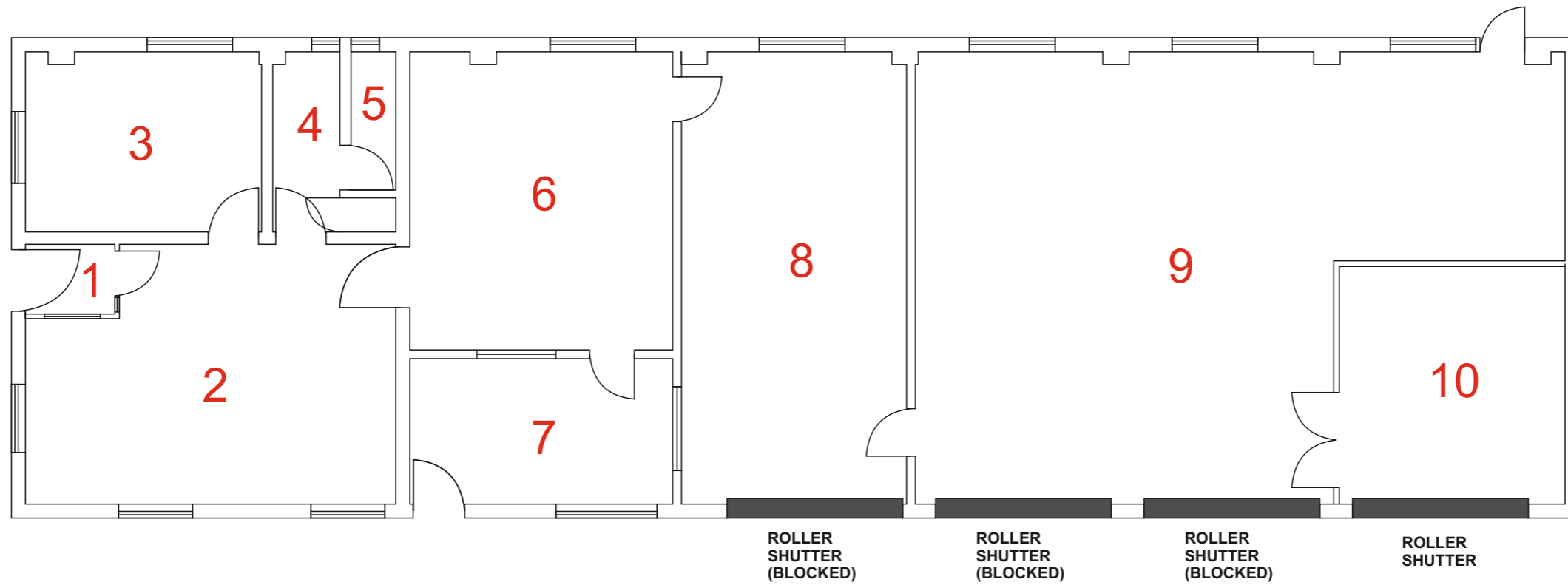


Plate 48: Northwest aspect, room 10

Room 10 is an area partitioned from room 9. This room houses a steel roller shutter door to the northeast. A modern double door is present leading into room 9. The lower part of the wall partitioning room 9 from room 10 is of painted concrete block. The upper part is of sheet material, with a high level internal window to the southwest wall.



Plate 49: Southwest aspect, room 10



Blue Bird Park

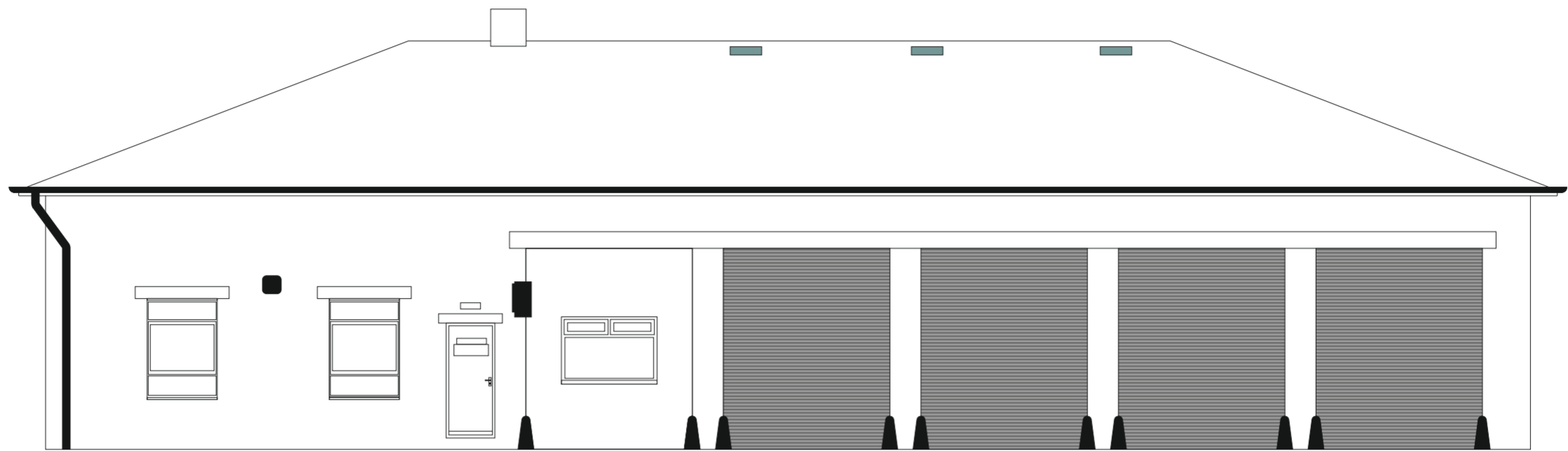
Transport Building Floor Plan



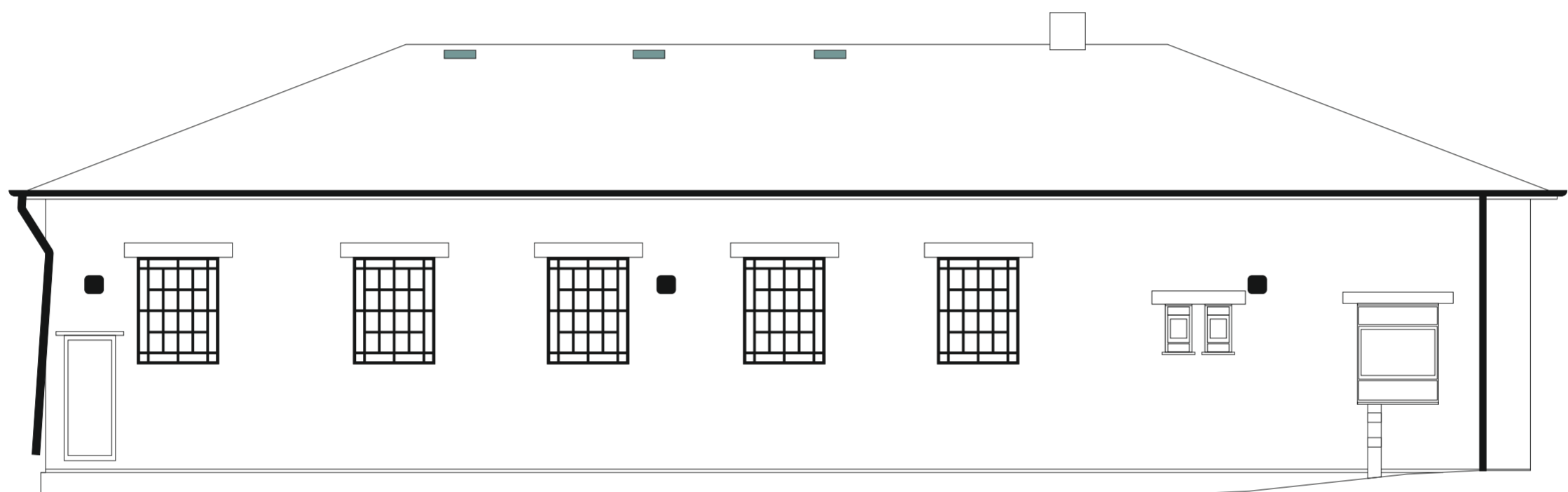
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Scale @A3 1:100

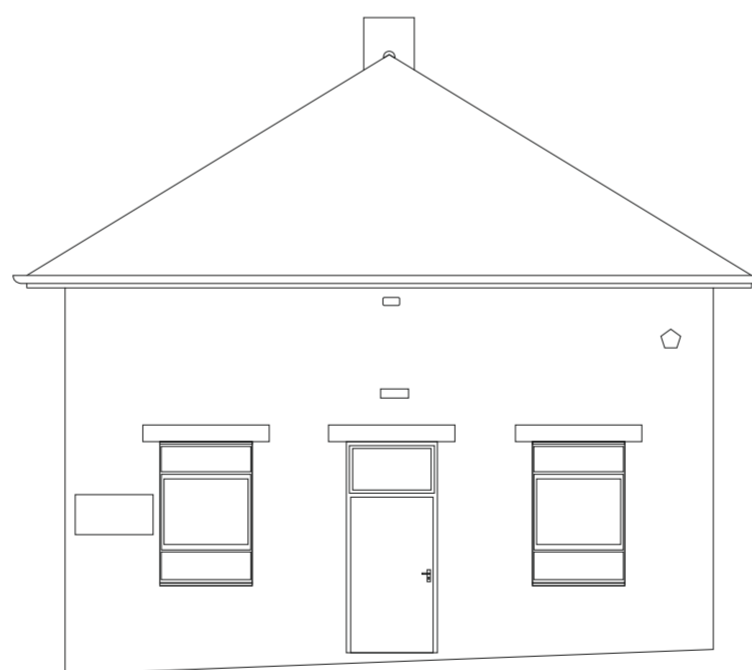




Northeast



Southwest



Southeast

Blue Bird Park
Transport Building Elevations

Scale @A3 1:100

